



**City of Libby
Libby Montana**
www.cityoflibby.com

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**Lights/Streets/Sidewalks Committee Agenda
Wednesday, January 7, 2026 @ Noon
City Hall, Council Chambers**

- Call Meeting to Order
 1. Discussion and decision: Sidewalk program guidelines to evaluate the cost sharing and possibility of putting out for bid.
 2. Discussion and decision: Removal of stop signs at Idaho and 6th. Original placement due to buses serving Asa Wood when operated as a school.
 3. Review and discuss Mineral Ave. Master Plan.
 4. Other items.
- Public Comment
- Adjournment

The manner of Addressing Committee:

- Each person, not a Committee member, shall address the Committee at the time designated in the agenda or as directed by the Committee, by stepping to the podium or microphone, giving that person's name and address in an audible tone of voice for the record, unless further time is granted by the Committee, shall limit the address to the Committee to three minutes.
- All remarks shall be addressed to the Committee as a body and not to any member of the Committee or Staff with no personal remarks allowed.
- No person, other than the Committee and the person having the floor, shall be permitted to enter any discussion either directly or through a member of the Committee, without the permission of the Committee Chair.
- Any person making personal, impertinent, or slanderous remarks or who shall become boisterous or disruptive during the committee meeting shall be forthwith barred from further presentation to the committee by the committee chair unless permission to continue is granted by a majority vote of the committee.

ATTENTION:

To access this meeting electronically with ZOOM,
Dial: 253-215-8782
Meeting ID: 4042719951
Password: 151041
Posted: 01/05/26



Downtown Mineral Avenue Master Plan

Libby, Montana

December 2025

CONTENTS



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Acknowledgments



Project Funded By

Montana Department of Commerce Community Technical Assistance Program (CTAP)

Ann Schwend

Project Support Provided By

City of Libby

Mayor Peggy Williams

Technical Assistance and Report Preparation Provided By

WGM Group

Kate Dinsmore, PLA

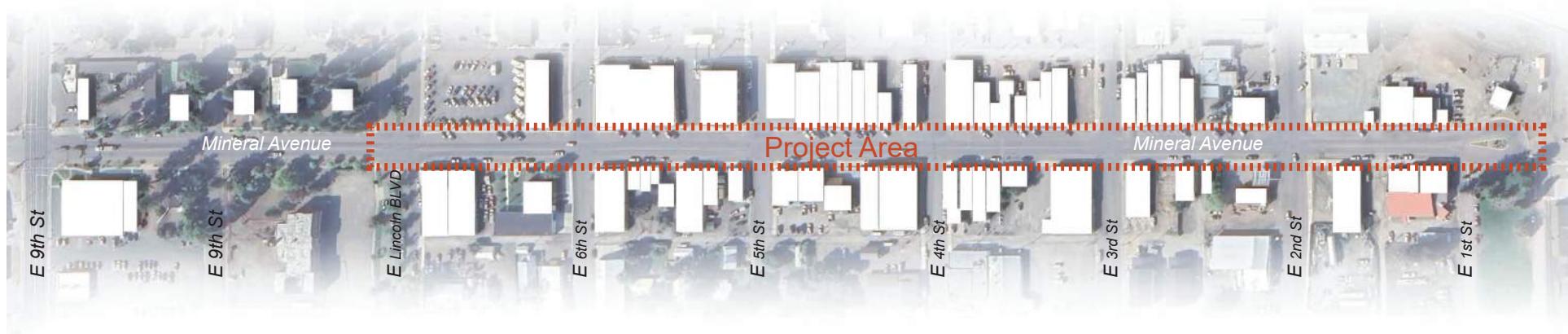
Ryan Sadowski, PE

Introduction

The Downtown Mineral Avenue Master Plan creates a vision for Downtown Libby. The plan includes phased improvements to support local businesses, providing spaces for additional programming and events to bring people Downtown, while utilizing a variety of funding sources. The phases are set up with the first phase being the easiest to implement and acting as a catalyst to build momentum for additional improvements. The plan includes intersection improvements for improved safety, aesthetic improvements such as sidewalk, lighting, signage, and facade enhancements, and additional spaces--parklets, a park, and a plaza--for people to gather and enjoy Mineral Avenue.

Project Area

The project area includes Mineral Avenue from E Lincoln Boulevard, or E 7th Street, past E 1st Street near the BNSF Train Depot. Mineral Avenue is Libby's Downtown commercial district with shopping, dining, and offices.



Community Input

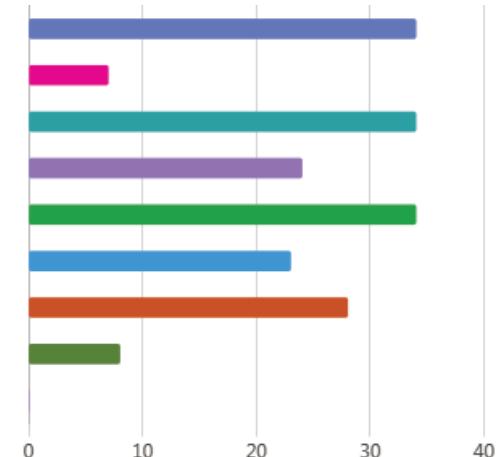
Initial feedback on Mineral Avenue was gathered on July 17-18, 2025 at the Farmers Market and the Libby Ambulance Barn. Staff from WGM Group, CTAP, and the City of Libby were available to discuss Mineral Avenue with residents, property owners, and business owners to understand what people like and dislike about Mineral Avenue as well as their vision for Mineral Avenue.

A survey was available and the responses are shown below.



Survey response: How do you use Mineral Avenue?

Walking	34
Biking	7
Driving	34
Parking	24
Shopping / Dining	34
Socializing	23
Attending events	28
Passing through	8
Other	0



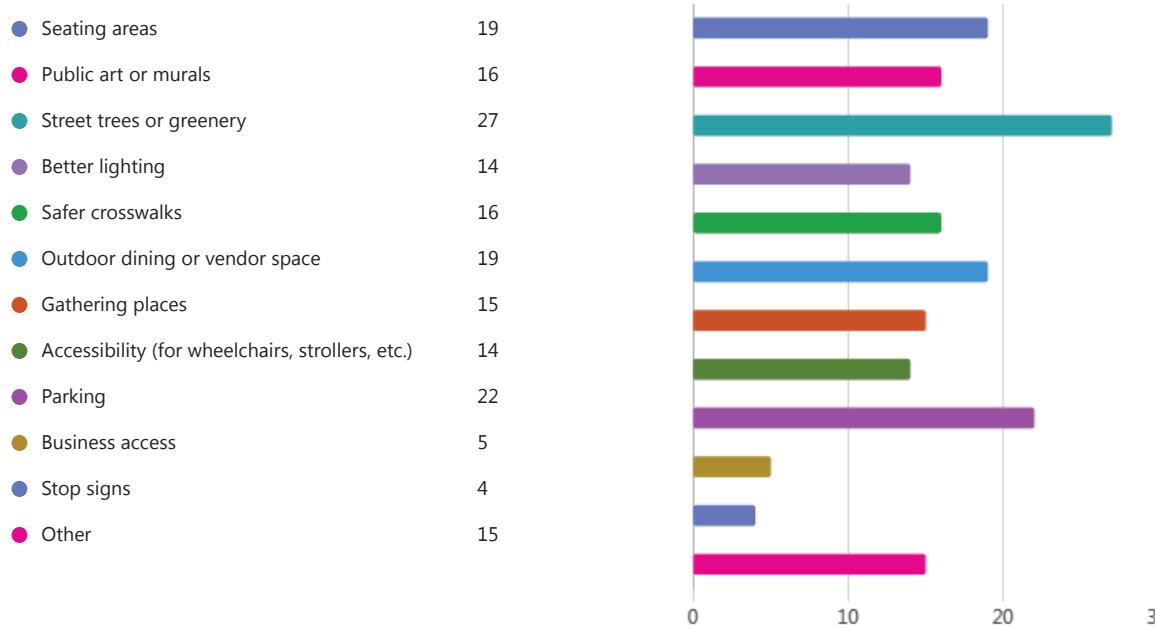
9 respondents (25%) answered businesses for this question.

coffee shops new businesses New mid businesses and new businesses are ones
new businesses downtown feel Eagle shops small revamps of business
downtown hand shops variety business owners towns business and landscaping types of businesses
new merchandise new variety new variety new variety new variety

Community Input

Survey Responses

What do you think is missing or could be improved on Mineral Avenue?



How would you score the overall safety on Mineral Avenue?



What kind of visual character should Mineral Avenue reflect?



Community Input

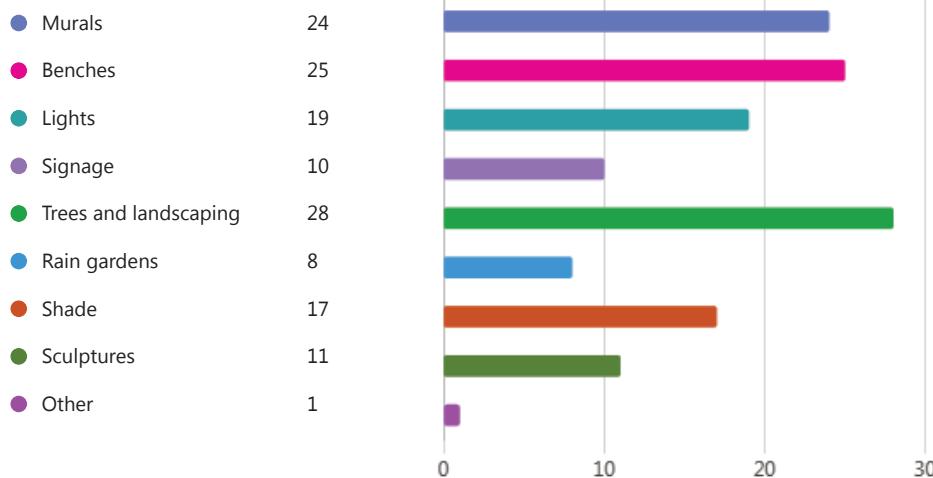
Survey Responses

If there are any particular areas or intersections on Mineral Avenue that you would prioritize for safety enhancements please let us know:

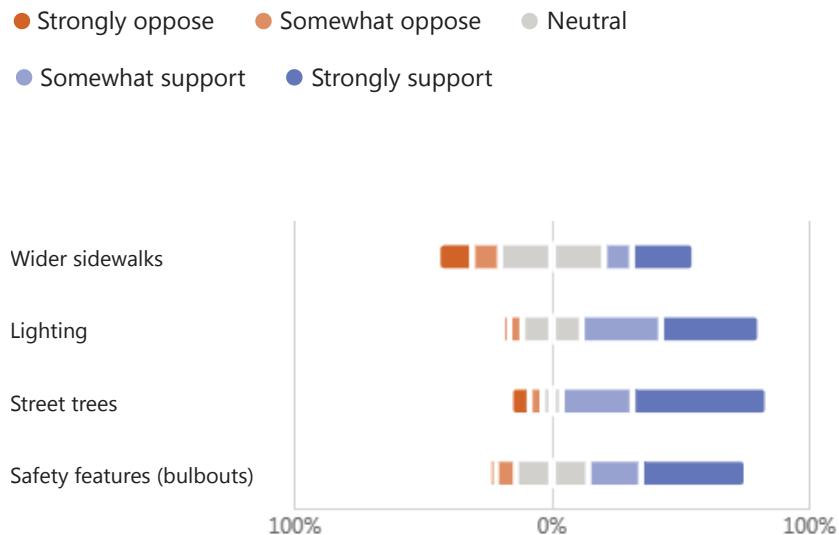
6 respondents (30%) answered Stop Signs for this question.

6th Street Non stop signs and change signs are newish Bank intersection
Wheelchair ramps white STOP street crosswalks signs have been great
better visibility way stops people parking way down the street
parking spaces intersections change of speed intersections and crosswalks

What features are reflective of the community?



What level of support would you give to reducing parking or driving lane widths to reallocate space to the following amenities?



Community Input

Survey Responses

RESIDENT CONCERN ABOUT FUTURE IMPROVEMENTS OF MINERAL AVENUE

1. It has to fit what folks want not what admin or PR thinks it should have. a quiet easier pace and a calm feel downtown. Just places to gather and chat and watch people.
2. Making sure that what is done is sustainable. stay with one vision.
3. Need to not be from mills. local grants funding is a must.
4. Don't make the street too narrow
5. Walkability and ability to expand and grow.
6. None. go for it!
7. Don't mess with parking. We did a recent design in favor of lots. would be much less convenient with time involved to shop downtown
8. Investing in a dying area with Port development taking over.
9. That a vocal minority take over the narrative
10. That existing merchants will be forced to upgrade. or that state taxpayer dollars will be used here
11. Cost to business
12. Safety & traffic signage
13. Business buy-in
14. That they won't be done!!

15. Business support
16. Anything would be an improvement so I don't see anything being a concern
17. We need to draw more attention from HWY 2
18. Cost
19. I love change
20. Cost
21. Not try to mimic other communities who definitely have more money in the area
- Keep to our roots
22. Cost
23. Clear communication with the community
24. Affordability, the communities who definitely have more money in the area
- Keep to our roots.
25. Taxes
26. Need more dining
27. Safety yield lights instead of stop signs placed on 6th Street and 3rd Street
28. Animal tracks & Big Foot tracks
29. Storefronts who do not wish to participate
30. The benefits of improvements going unrecognized by residents



Residents' vision for Mineral Avenue

Community Input

Survey Responses

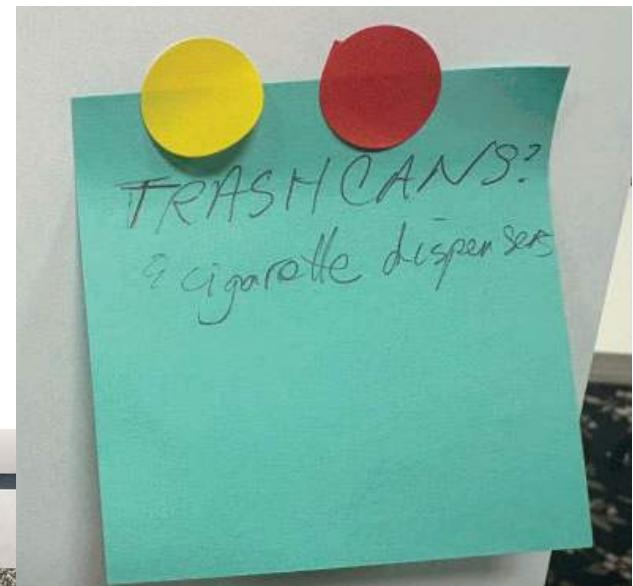
RESIDENT COMMENTS

1. New lights and sidewalks. angled parking please.
2. Use “peninsulas” for trees
3. Move redundant business out of Mineral Ave.
4. I prefer each building owner to decide the improvements they desire
5. MUTCD Traffic Analysis
6. Space for gathering downtown that is not business-dependent would be awesome. Native plants should be emphasized when selecting greenery.
7. A better Libby
8. Access to water
9. Glad you’re doing this!
10. Trees could not only add natural charm but also add shade. Benches might help encourage shoppers?
11. Thank you for this project :)
12. Signs needed for downtown shopping in Mineral
13. Crosswalks instead of stop signs. Have public meetings after 6pm for business owners to attend
14. While businesses have a say in what their storefront looks like, I worry that some of the more vengeful organizations may blatantly disregard.

Community Input

A meeting was held on November 12, 2025 to present the proposed improvements to the community. Attendees were asked to vote on options and provide comments. The results and comments are shown below.

The sticker dots indicated a preference for the more modern style of the Corten steel planter or fiberglass round planter.



Which planter option do you prefer?

Please place a dot sticker by your preferred option. These will be placed at the 4th Street and 6th Street intersections and available for use outside businesses.



Corten Steel Planter



Fiberglass Round Planter (Colors Available)



Barrel Planter



Textured Concrete Planter



Community Input

There was a preference for angled parking on both sides of the street with sidewalk bulb-outs at key intersections.

Which option do you prefer?
Please place a dot sticker by the rendering of your preferred option.

Angled and parallel parking, no sidewalk bulb-outs

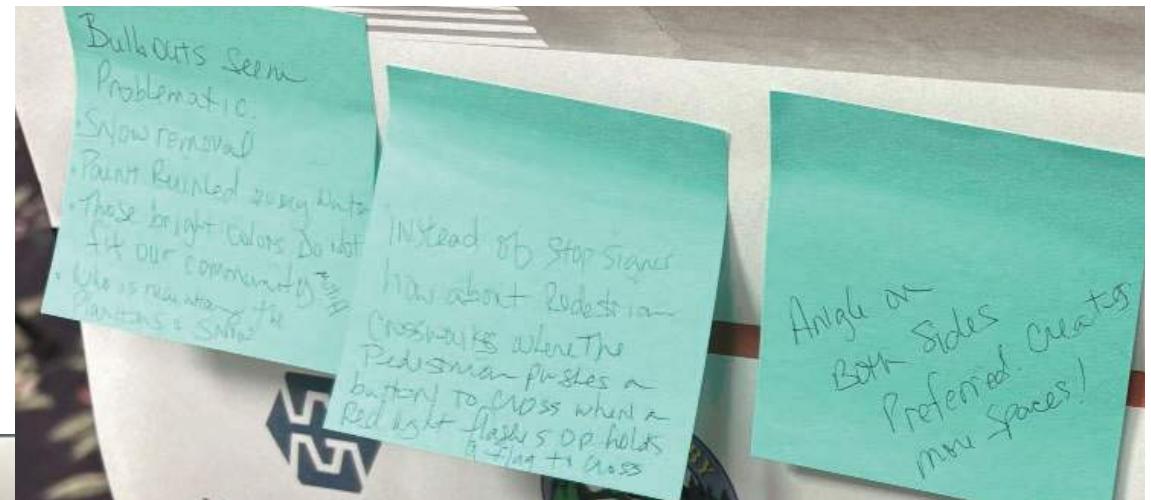


Angled parking on both sides of the street, no sidewalk bulb-outs

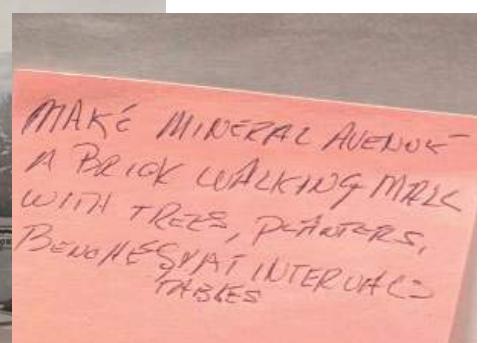


MINERAL AVE
Improvements Opportunities Project
Phase 2

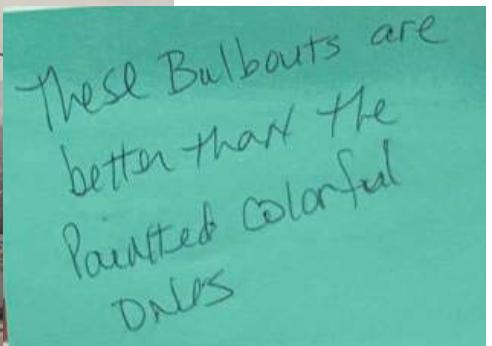
Project funded by the
Montana Department of Transportation



Angled and parallel parking, with sidewalk bulb-outs

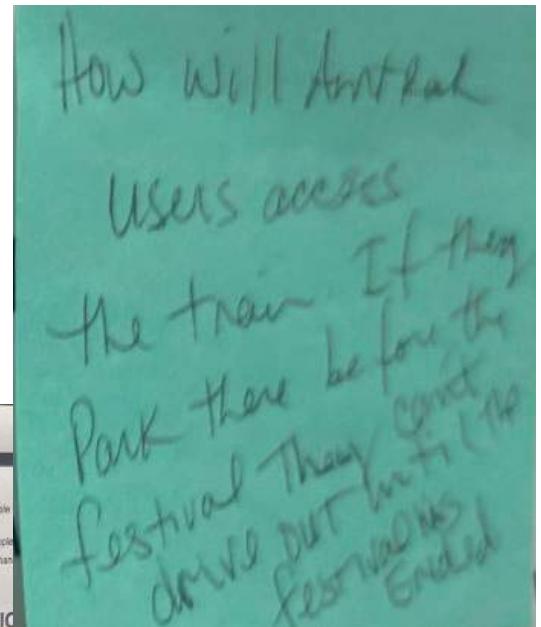
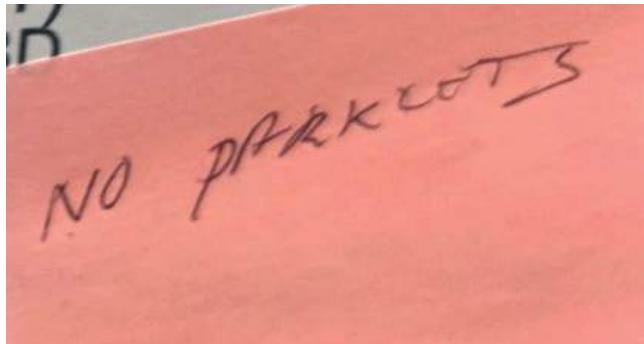


Angled parking on both sides of the street, with sidewalk bulb-outs



Community Input

The sticker dots below indicate support for the street and intersection improvements and the street plaza.



Phase 3 Improvements

- Street Plaza: A festival and pop-up market space at the terminus of Mineral Avenue and along 1st Street will transform to a flexible space that accommodates traffic but can also be a pedestrian-only space for events. This includes improvements along 1st Street so traffic can be rerouted during events in the plaza.
- 9th Street Wayfinding: Aesthetic improvements will be introduced at the 9th Street intersection to create a change in rhythm and pattern along 9th Street so visitors intuitively recognize that Mineral Avenue is different than other streets. This can be accomplished through landscaping, public art, and signage.
- Business Sign, Awning, and Facade Program: This program will provide financial assistance for business owners for improving their building facades. This can include signage, murals, awnings, and other facade improvements.
- Programming: Events, festivals, and visitor activities are encouraged on Mineral Avenue to bring vibrancy to the area. People are attracted to lively and exciting spaces. Creative ideas to use the proposed spaces are key.

COST AND FUNDING OPPORTUNITIES

Funding for Phase 3 improvements has not been determined yet, but it will likely include a combination of grant funding, private funding, and City funding. This could include the formation of a Business Improvement District that would be funded by assessments to property owners.

- Street Plaza: \$450,000
- 9th Street Wayfinding: \$15,000
- 1st Street Improvements: TBD
- Facade Program: TBD

• Total Phase 3 Costs: \$465,000 + 1st Street Improvements + Facade Program

These are conceptual costs that are subject to change during design.

BENEFITS

- Event space that brings people together and businesses
- Wayfinding to encourage people to visit
- Building improvements to enhance businesses

IMPLEMENTATION

- Cost
- Right-of-way limitations: BNSF owns the property at the terminus of Mineral. Improvements at 9th and Mineral will need to be coordinated with adjacent property owners.

Rendering of Mineral Avenue with All Proposed Improvements

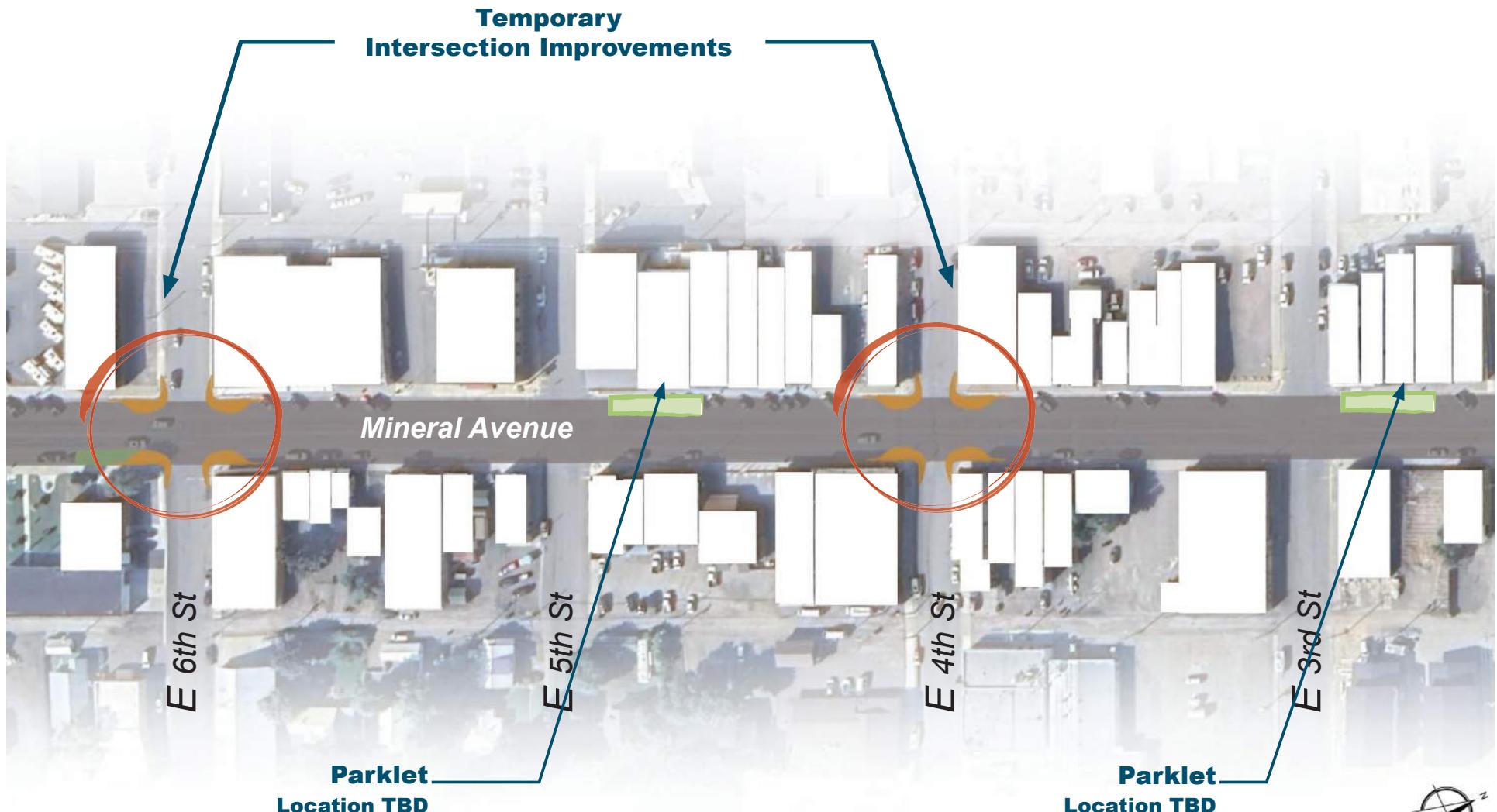
Master Plan Improvements

Phase 1 Improvements

Phase 1 includes temporary intersection improvements, planters, and parklets. The goal for Phase 1 is to implement small, low-cost improvements to get some quick, easy wins to build interest and momentum

for implementation of additional phases. This phase may also include the establishment of a facade improvement program to aid businesses in improving their store fronts. See Phase 3 for more information.

The intent is for these improvements is to spark interest and serve as a way to try out options before making a larger investment.



Master Plan Improvements

Phase 1 Improvements

TEMPORARY INTERSECTION IMPROVEMENTS

Improvements at 4th Street and 6th Street will be low cost, temporary improvements intended to provide traffic calming and safety improvements by reducing pedestrian crossing distance. These improvements will utilize delineators, paint, and planters. Paint colors and patterns can be simple or a community art project.



Intersection Examples



Planter Examples



Parklet Examples

Master Plan Improvements

Phase 1 Improvements

COST AND FUNDING OPPORTUNITIES

- Temporary Intersection Improvements: \$10,000
- Planters: \$10,000-\$15,000
- Parklets: \$10,000 - \$20,000 each
- **Total Phase 1 Costs:** \$50,000

Phase 1 costs are planned to align with the funding available through the Main Street Grant.

BENEFITS

- Low cost improvements to assess intersection improvements and determine if permanent improvements are desirable.
- With the shortened crossing distance and traffic calming, stop signs may no longer be needed at these intersections.
- Parklet will expand pedestrian space and provide interest on the street.
- Quick, easy-to-implement improvements to begin unifying Mineral Avenue.

IMPLEMENTATION CHALLENGES

- Intersection improvements are meant to be temporary. If the temporary improvements are effective, either the temporary improvements will eventually need to be replaced or permanent improvements will need to be installed.
- Planters outside of businesses will need to be maintained and watered by the business.
- Snow plow routes will be affected.



Rendering of Phase 1 improvements

Master Plan Improvements

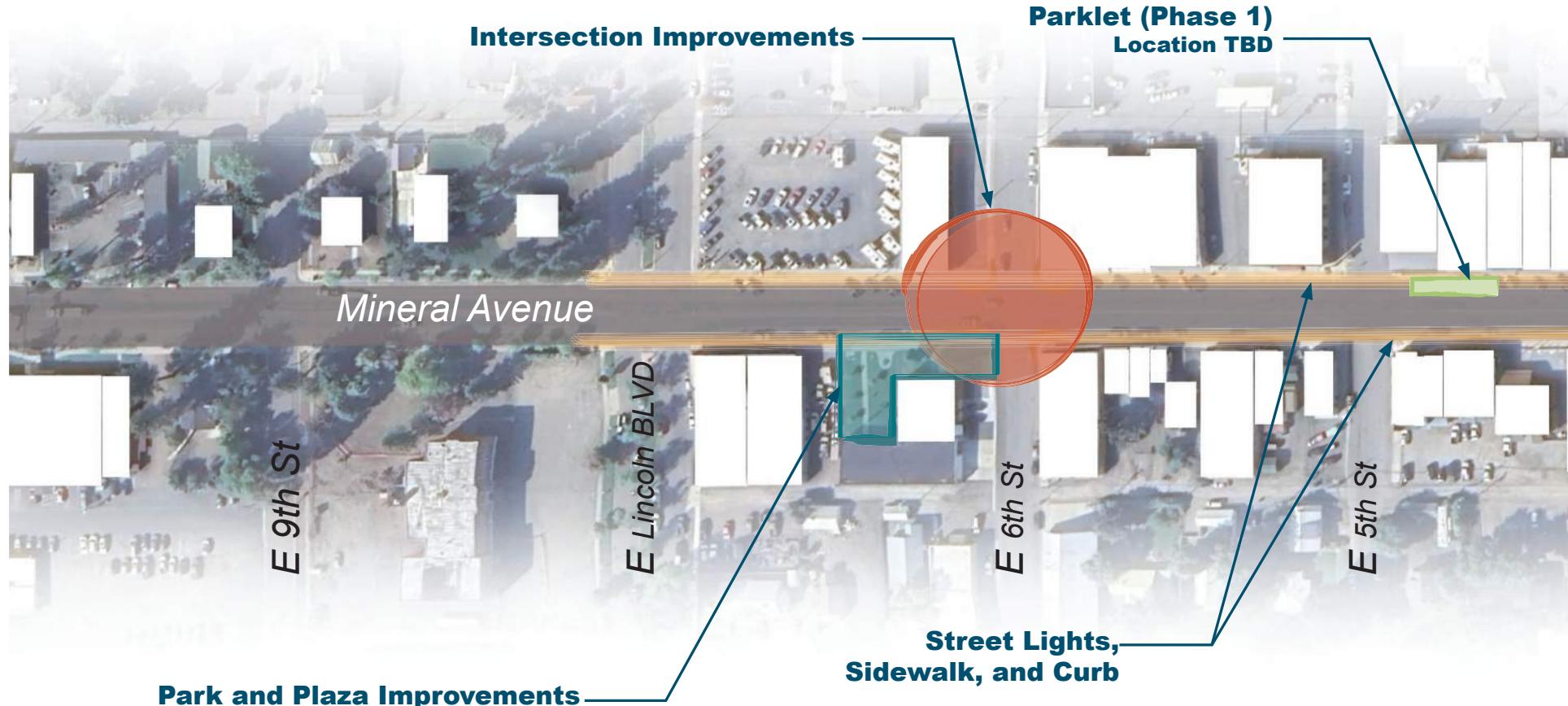
Phase 2 Improvements

Phase 2 includes permanent intersection improvements at 4th and 6th, park and plaza improvements outside the Libby Police Station, street lights, sidewalk, and curb. The goal for Phase 2 is to implement impactful enhancements to improve safety

and aesthetics. These improvements will provide long-term benefits to Mineral Avenue enhancing the street and businesses.

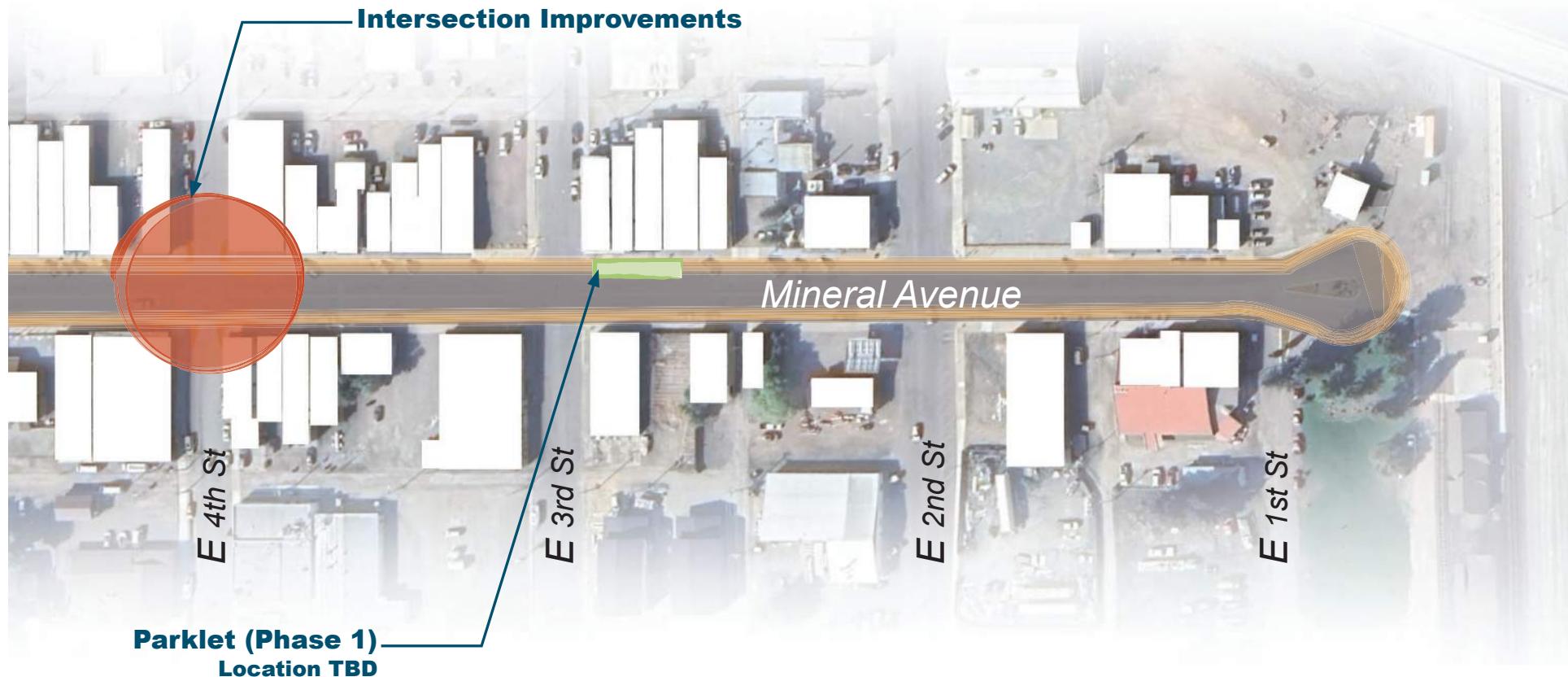
This phase also includes changing the on-street parking configuration to be all angled

parking. This adds parking capacity along Mineral Avenue while providing traffic calming. The angled parking will make the street feel tighter which causes people to drive slower.



Master Plan Improvements

Phase 2 Improvements



Master Plan Improvements

Phase 2 Improvements

STREET LIGHTS

Decorative street lighting will be installed from Lincoln to 1st aligning with the existing street lights from 9th to Lincoln. The pedestrian-scale lights will add character and unify Mineral Avenue while providing for hanging flower baskets, flags, and banners.

SIDEWALKS AND CURB

Safe, accessible, pedestrian space will be provided with new sidewalks and curb. This will remove unused driveway curb cuts and provide ADA-compliant ramps at intersections.

PARK AND PLAZA

Improvements to the existing area around the Libby Police Department will provide a space for public gatherings, to sit, or enjoy lunch on Mineral Avenue.

BULB-OUTS

A bulb-out or curb extension creates a narrower roadway width, enhancing pedestrian safety by reducing crossing distances and improving visibility for both pedestrians and drivers. This creates more space for pedestrian-friendly amenities at intersections such as sidewalks, landscaping and street furniture. These are proposed at the 4th Street and 6th Street intersections.

PARKING

There was a preference to change the parking configuration to all angled parking during the second public meeting. Angled parking on both sides of the street increases parking spaces and provides traffic calming slowing down traffic on Mineral Avenue. The trade off is that it can be difficult to see oncoming traffic for those backing out of a parking space.

LANDSCAPING

Vegetation and color will be provided with hanging flower baskets on the light poles, at the bulb-outs, and at the Police Department Park. Additional trees and landscaping can be added at opportune locations. There are challenges with installing street trees along the entire length of Mineral Avenue because of the sidewalk width, building awnings, cost, and maintenance. Trees surrounded by concrete and asphalt have difficulty thriving due to limited soil volumes. Another option is for trees to be installed in landscape islands within the parking lane, but that would decrease available parking spaces and impact snow plowing as well as require additional curb. Trees also require maintenance that the City is not equipped to take on at this time.



Street Improvement Examples

Master Plan Improvements

Phase 2 Improvements

COST AND FUNDING OPPORTUNITIES

The Montana Pilot Community Tourism Grant has been identified as a potential source of funding for Phase 2. Additional funding sources could include the City's Lighting Program or a City revolving loan.

- Street Lighting: \$1,275,000
- Sidewalk: \$750,000
- Curb: \$165,000
- Demolition, ADA Ramps, Irrigation: \$750,000
- Hanging Flower Baskets: \$15,000
- Bulb-outs: \$480,000
- Park/Plaza: \$150,000
- **Total Phase 2 Costs:** \$3,585,000

These are conceptual costs that are subject to change during design.

BENEFITS

- Unifying elements that provide aesthetic continuity
- Safety enhancements at crossings
- Gathering space for small events

IMPLEMENTATION CHALLENGES

- Bulb-outs would alter snow plow operations
- Cost
- Disruption to businesses during construction



Rendering of Phase 2 improvements

Master Plan Improvements

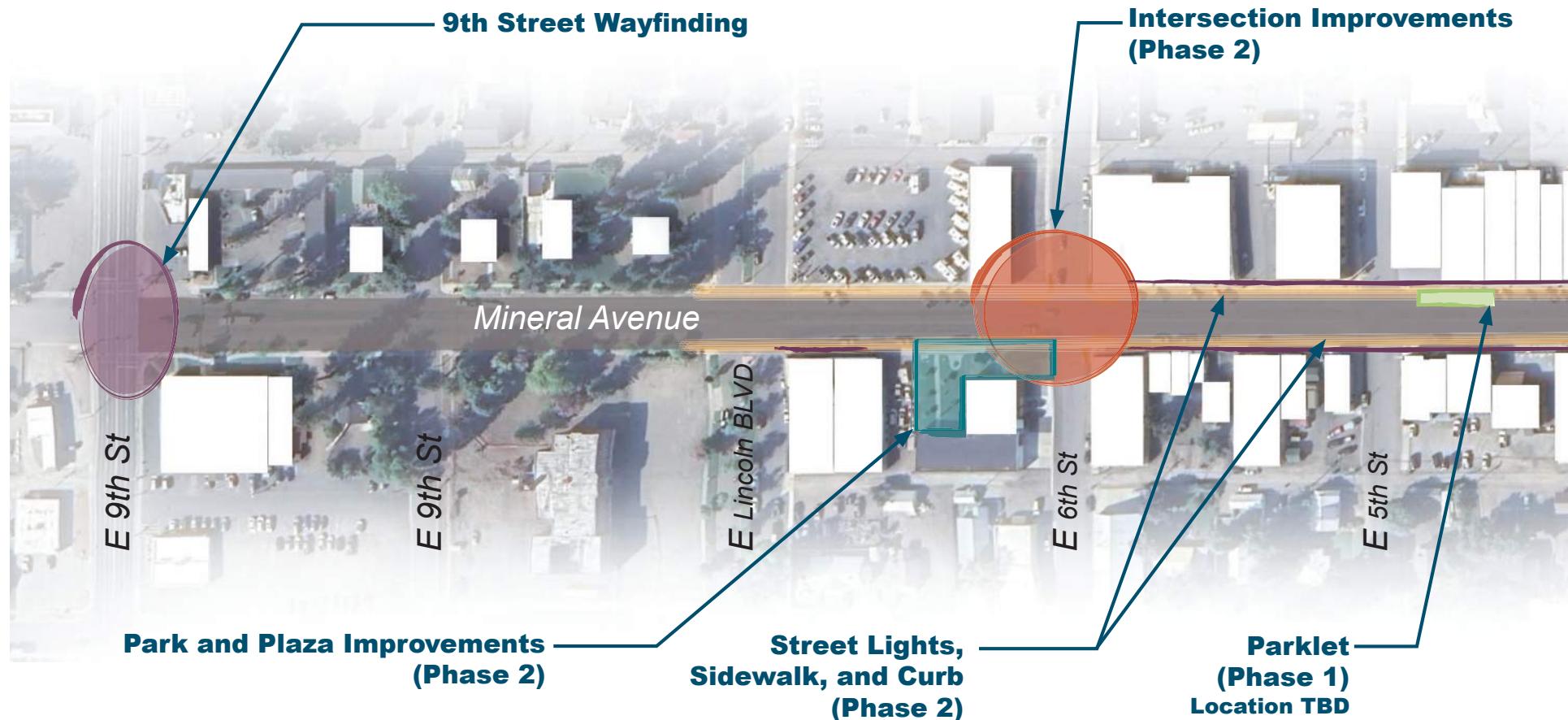
Phase 3 Improvements

Phase 3 includes 9th Street wayfinding improvements to intuitively direct traffic to Mineral Avenue, a street plaza at the north end of Mineral Avenue to provide space for events, and a business sign, awning, and facade program. The business sign, awning, and facade program should be established as early

as possible so it could be implemented as early as Phase 1.

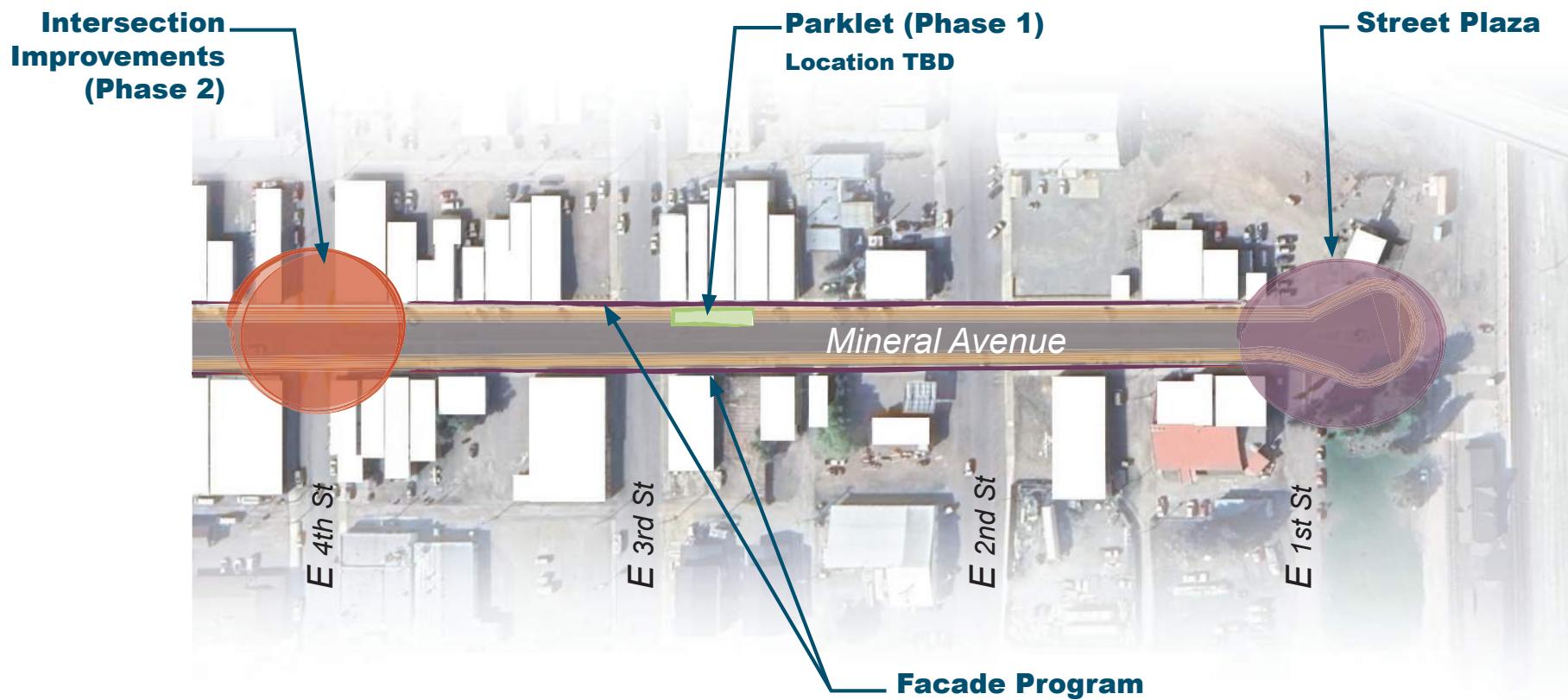
The street plaza creates a designated space for events without blocking traffic on Mineral Avenue or reducing visibility to store fronts.

Phase 3 also includes improvements to 1st Street as this street would need to accommodate additional traffic with the implementation of the street plaza.



Master Plan Improvements

Phase 3 Improvements



Master Plan Improvements

Phase 3 Improvements

STREET PLAZA

A festival and pop-up market space at the terminus of Mineral Avenue and along 1st Street will transform to a flexible space that accommodates traffic but can also be a pedestrian-only space for events. This includes improvements along 1st Street so traffic can be rerouted during events in the plaza.

9TH STREET WAYFINDING

Aesthetic improvements will be introduced at the 9th Street intersection to create a change in

rhythm and pattern along 9th Street so visitors intuitively recognize that Mineral Avenue is different than other streets. This can be accomplished through landscaping, public art, and signage.

BUSINESS SIGN, AWNING, AND FAÇADE PROGRAM

This program will provide financial assistance for business owners for improving their building facades. This can include signage, murals, awnings, and other facade improvements.

PROGRAMMING

Events, festivals, and visitor activities are encouraged on Mineral Avenue to bring vibrancy to the area. People are attracted to lively and exciting spaces. Creative ideas to use the proposed spaces are key.



Rendering of Street Plaza at Mineral Avenue Terminus

Master Plan Improvements

Phase 3 Improvements

COST AND FUNDING OPPORTUNITIES

Funding for Phase 3 improvements has not been determined yet, but it will likely include a combination of grant funding, private funding, and City funding. This could include the formation of a Business Improvement District that would be funded by assessments to property owners.

- Street Plaza: \$450,000
- 9th Street Wayfinding: \$15,000
- 1st Street Improvements: TBD
Improvements may include curb, sidewalk, paving, etc.
- Façade Program: TBD
- **Total Phase 3 Costs:** \$465,000 + 1st Street Improvements + Façade Program

These are conceptual costs that are subject to change during design.

BENEFITS

- Event space that brings people to Mineral Avenue while limiting disruption to traffic patterns and businesses
- Wayfinding to encourage people to visit Mineral Avenue
- Building improvements to enhance the aesthetics of Mineral Avenue and support local businesses

IMPLEMENTATION CHALLENGES

- Cost
- Right-of-way limitations: BNSF owns the property at the terminus of Mineral. Improvements at 9th and Mineral will need to be coordinated with adjacent property owners.



Master Plan Improvements

Phase 3 Improvements



Rendering of all proposed improvements

TOTAL COSTS OF IMPROVEMENTS

~\$4,100,000 + 1st Street Improvements and Facade Program

Phase 1

- Temporary Intersection Improvements: \$10,000
- Planters: \$10,000-\$15,000
- Parklets: \$10,000 - \$20,000 each
- **Total Phase 1 Costs:** \$50,000

Phase 2

- Street Lighting: \$1,275,000
- Sidewalk: \$750,000
- Curb: \$165,000
- Demolition, ADA Ramps, Irrigation: \$750,000
- Hanging Flower Baskets: \$15,000
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Phase 3

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- 9th Street Wayfinding: \$15,000
- 1st Street Improvements: TBD
Improvements may include curb, sidewalk, paving, etc.
- Facade Program: TBD
- **Total Phase 3 Costs:** \$465,000 + 1st Street Improvements + Facade Program



CITY OF LIBBY

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| PHONE 406-293-2731 | FAX 406-293-4090 | WEBSITE: www.cityoflibby.com

City of Libby Sidewalk Policy

To help encourage property owners in the city to replace deteriorating sidewalks, the City of Libby has the following policy:

Property Owners Responsibility

1. Contact the city and pick up a permit for sidewalk replacement. There is no charge for the permit. Permit must be approved by the City Administrator or Streets Supervisor.
2. Contact a contractor to perform the work or, if qualified, replace the sidewalk themselves following the specifications for sidewalks in City Code. This information is provided with the sidewalk permit.
3. Contact U-DIG before beginning work for utility locates and ensure a safe work environment throughout the entire project.

City Responsibility

1. Remove old sidewalk material and dispose of it once it is separated from abutting sidewalks and is broken into manageable pieces.
2. Haul in $\frac{3}{4}$ " minus crush to be compacted for the base under the sidewalk.
3. Reimburse the property owner \$10.00 a linear foot on the replacement. (Example: 100 linear feet would be \$1000.00)
4. Reimburse the commercial property owner, with 6 feet or wider sidewalks, \$15.00 a linear foot on the replacement. (Example: 100 linear feet would be \$1500.00)

The city is responsible for the curb and gutter and the end of each block where the handicap ramp accesses the street. The city's maximum sidewalk reimbursement is limited to \$10,000 a fiscal year and operates on a first come first serve basis.

Please read the permit carefully and follow the guidelines within. Contact the Building Inspector, City Administrator, or Streets Supervisor at each of the three stages of the project for inspection. Once the project is completed return the completed permit and receipts to the city for reimbursement.

The City of Libby appreciates your interest in replacing sidewalks and if you have any questions, please call City Hall at 293-2731 or stop by for information.

Thank You,
City of Libby

Permit No. _____

**City of Libby
Sidewalk Permit**

Property Owner:

Name: _____

Address: _____

Phone/Email: _____

Contractor:

Name: _____

Address: _____

Phone/Email: _____

Lineal Feet 5' Wide = _____ Feet x \$10.00 = _____

Reimbursement 6'+ Wide = _____ Feet x \$15.00 = _____

Make Check payable to: _____

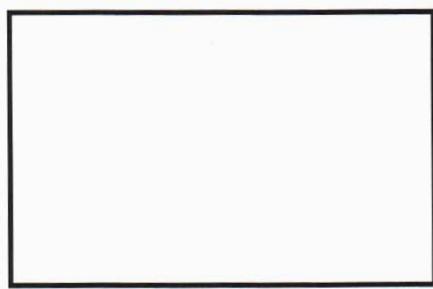
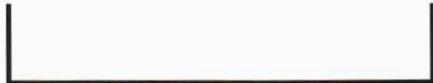
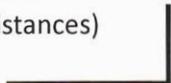
Curb and Gutter feet requested = _____ Approved = _____ By: _____

Drive Over feet requested = _____ Approved = _____ By: _____

Pedestrian Ramp required = _____ YES or NO _____

Specific Location:

(Include Distances)



Comments:

Approved by:

City Administrator or Streets Supervisor

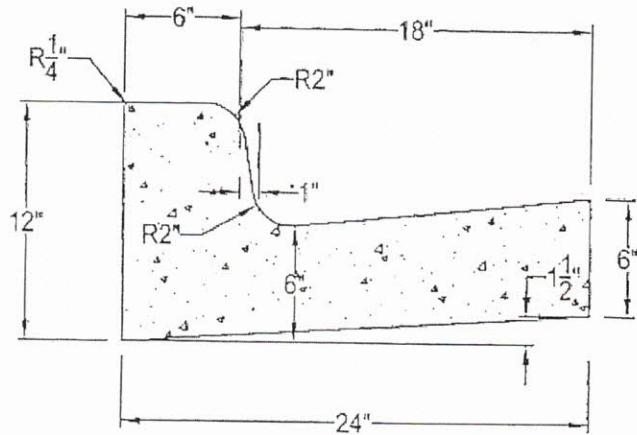
Date:

Phase Inspections (Initials):

Phase #1:

Phase #2:

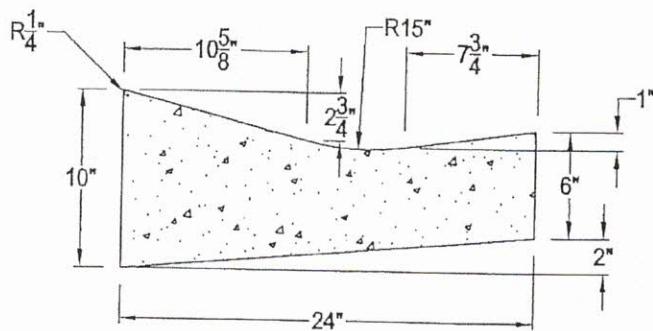
Final:



1. 1/2" EXPANSION JOINT MATERIAL SHALL BE PLACED AT EACH POINT OF CURVATURE AND POINT OF TANGENCY.
2. CONTRACTION JOINTS SHALL BE PLACED AT EVERY 15' OF CURB LENGTH AND SHALL HAVE A MINIMUM DEPTH OF 3/4" AND A MINIMUM WIDTH OF 1/8". CONTRACTION JOINTS SHALL BE CONSTRUCTED BY SAWING OR SCORING. A TOOL SHALL BE USED WHICH WILL LEAVE CORNERS ROUNDED AND DESTROY AGGREGATE INTERLOCK FOR THE SPECIFIED MINIMUM DEPTH.
3. EXPOSED EDGES SHALL BE FINISHED TO A RADIUS OF 1/4".
4. CONCRETE SHALL BE M-4000 WITH 3/4" MAXIMUM AGGREGATE, MINIMUM 28-DAY STRENGTH OF 4000 PSI, 6% \pm 1 1/2% AIR ENTRAINMENT, AND MAXIMUM SLUMP OF 4".
5. INDIVIDUAL CONTRACTORS FORMS MAY VARY SLIGHTLY FROM THIS PATTERN. PATTERNS DIFFERING MATERIALLY FROM THE ABOVE DIMENSIONS SHALL BE SUBMITTED TO THE CITY FOR REVIEW.
6. FOUR INCHES OF CRUSHED GRAVEL BASE MATERIAL, -3/4" DIAMETER IS REQUIRED FOR THE CURB AND GUTTER FOUNDATION. THE BASE MATERIAL SHALL BE COMPACTED TO 95% DENSITY (\pm 3% OPTIMUM MOISTURE) PER AASHTO T-99.

CONSTRUCTION STANDARDS
CITY OF LIBBY

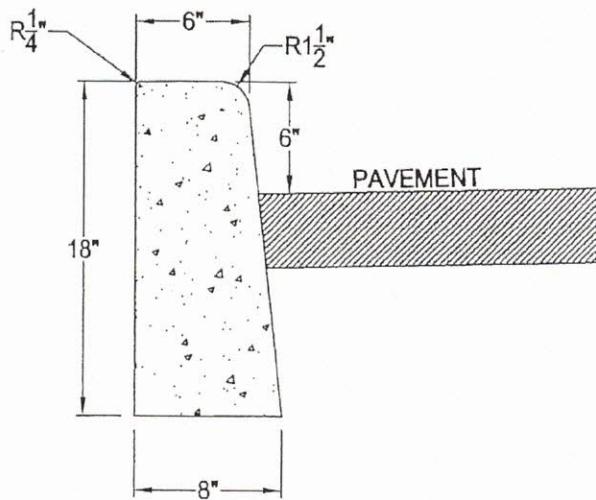
CURB AND GUTTER



1. 1/2" EXPANSION JOINT MATERIAL SHALL BE PLACED AT EACH POINT OF CURVATURE AND POINT OF TANGENCY.
2. CONTRACTION JOINTS SHALL BE PLACED AT EVERY 15' OF CURB LENGTH AND SHALL HAVE A MINIMUM DEPTH OF 3/4" AND A MINIMUM WIDTH OF 1/8". CONTRACTION JOINTS SHALL BE CONSTRUCTED BY SAWING OR SCORING. A TOOL SHALL BE USED WHICH WILL LEAVE CORNERS ROUNDED AND DESTROY AGGREGATE INTERLOCK FOR THE SPECIFIED MINIMUM DEPTH.
3. EXPOSED EDGES SHALL BE FINISHED TO A RADIUS OF 1/4".
4. CONCRETE SHALL BE M-4000 WITH 3/4" MAXIMUM AGGREGATE, MINIMUM 28-DAY STRENGTH OF 4000 PSI, 6% \pm 1 1/2% AIR ENTRAINMENT, AND MAXIMUM SLUMP OF 4".
5. INDIVIDUAL CONTRACTORS FORMS MAY VARY SLIGHTLY FROM THIS PATTERN. PATTERNS DIFFERING MATERIALLY FROM THE ABOVE DIMENSIONS SHALL BE SUBMITTED TO THE CITY FOR REVIEW.
6. FOUR INCHES OF CRUSHED GRAVEL BASE MATERIAL, -3/4" DIAMETER IS REQUIRED FOR THE CURB AND GUTTER FOUNDATION. THE BASE MATERIAL SHALL BE COMPACTED TO 95% DENSITY (\pm 3% OPTIMUM MOISTURE) PER AASHTO T-99.
7. THIS CURB DOES NOT MEET HANDICAPPED ACCESS REQUIREMENTS AND SHALL NOT BE USED FOR ACCESS RAMPS.

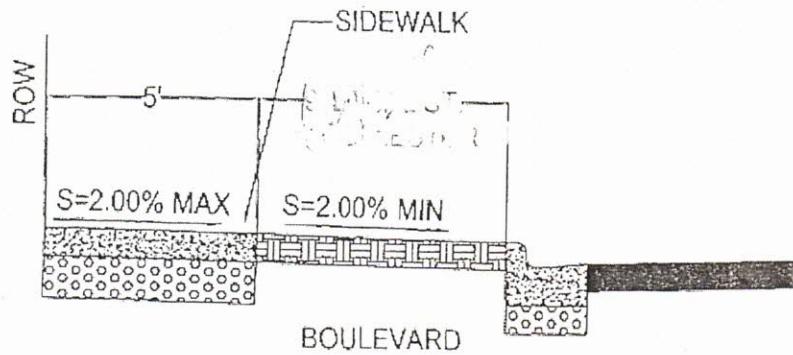
CONSTRUCTION STANDARDS
CITY OF LIBBY

DRIVE OVER CURB & GUTTER



1. 1/2" EXPANSION JOINT MATERIAL SHALL BE PLACED AT EACH POINT OF CURVATURE AND POINT OF TANGENCY.
2. CONTRACTION JOINTS SHALL BE PLACED AT EVERY 15' OF CURB LENGTH AND SHALL HAVE A MINIMUM DEPTH OF 3/4" AND A MINIMUM WIDTH OF 1/8". CONTRACTION JOINTS SHALL BE CONSTRUCTED BY SAWING OR SCORING. A TOOL SHALL BE USED WHICH WILL LEAVE CORNERS ROUNDED AND DESTROY AGGREGATE INTERLOCK FOR THE SPECIFIED MINIMUM DEPTH.
3. VISIBLE EDGES SHALL BE FINISHED TO A RADIUS OF 1/4", UNLESS OTHERWISE NOTED.
4. CONCRETE SHALL BE M-4000 WITH 3/4" MAXIMUM AGGREGATE, MINIMUM 28-DAY STRENGTH OF 4000 PSI, 6% \pm 1 1/2% AIR ENTRAINMENT, AND MAXIMUM SLUMP OF 4".
5. INDIVIDUAL CONTRACTORS FORMS MAY VARY SLIGHTLY FROM THIS PATTERN. PATTERNS DIFFERING MATERIALLY FROM THE ABOVE DIMENSIONS SHALL BE SUBMITTED TO THE CITY FOR REVIEW.
6. FOUR INCHES OF CRUSHED GRAVEL BASE MATERIAL, -3/4" DIAMETER IS REQUIRED FOR THE CURB FOUNDATION. THE BASE MATERIAL SHALL BE COMPAKTED TO 95% DENSITY (\pm 3% OPTIMUM MOISTURE) PER AASHTO T-99.
7. ONLY ALLOWED WITH SPECIFIC APPROVAL OF CITY

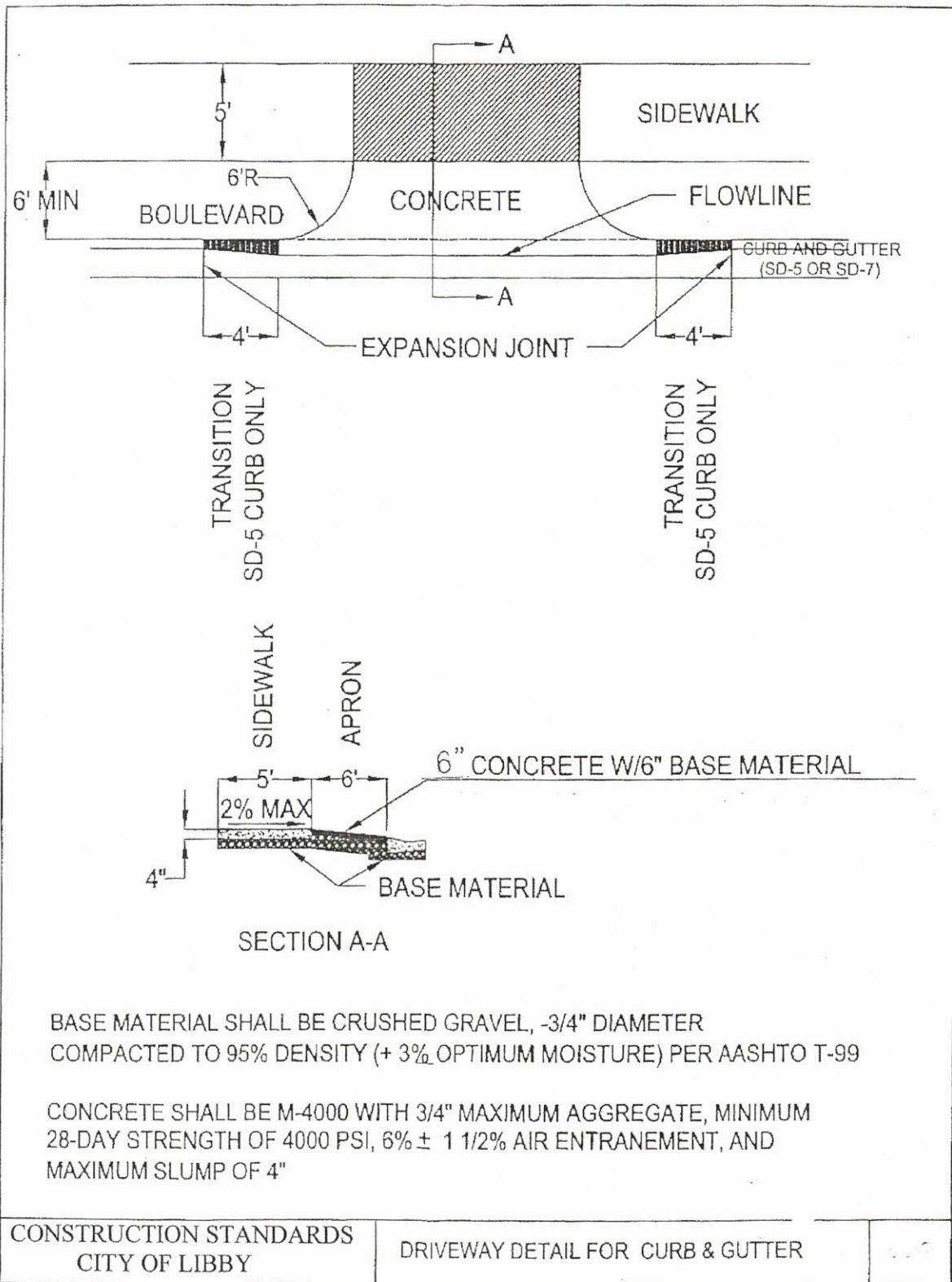
CONSTRUCTION STANDARDS CITY OF LIBBY	STRAIGHT CURB	
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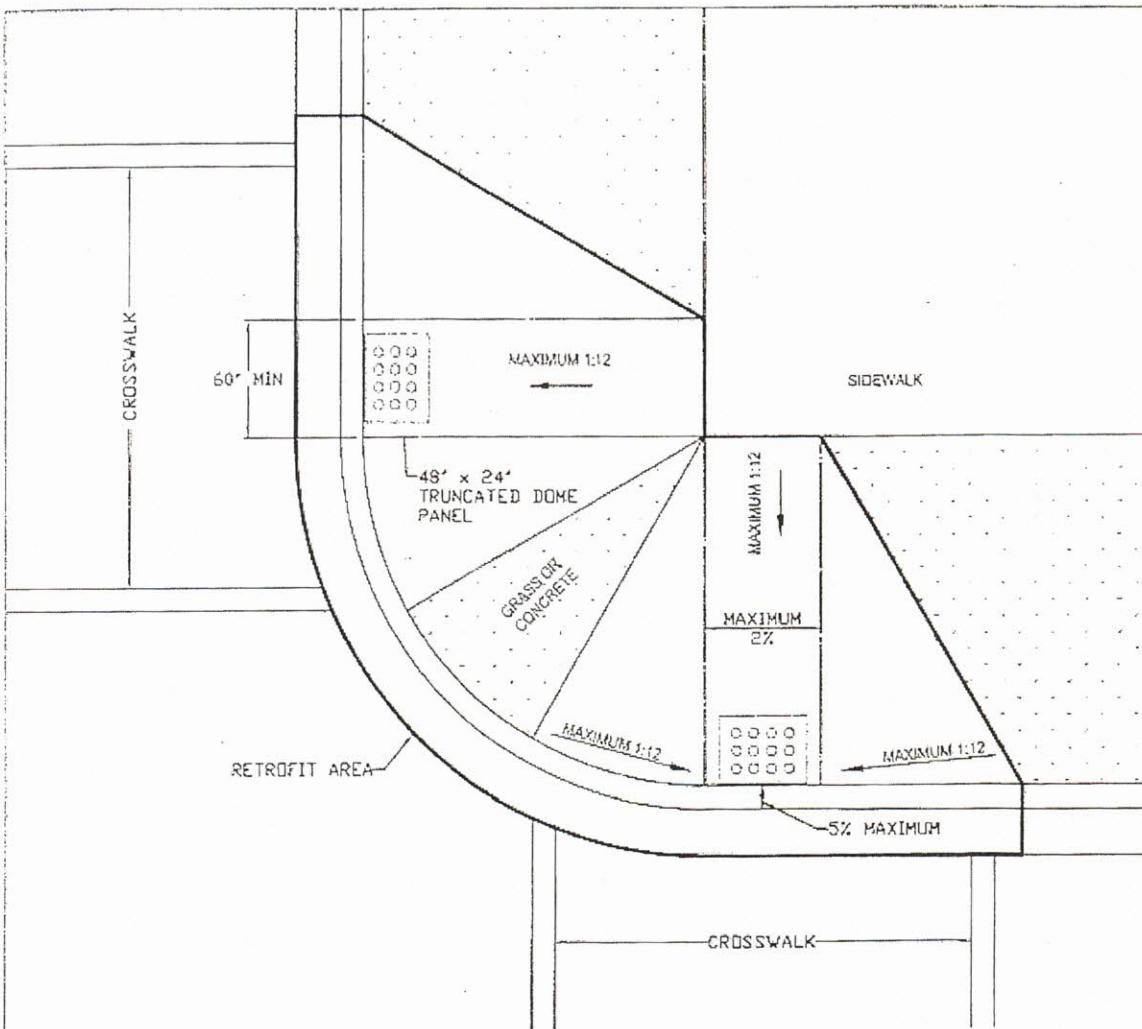


1. PRE-FORMED 1/2" EXPANSION JOINT MATERIAL MEETING THE REQUIREMENTS OF AASHTO M-213 SHALL BE PLACED AT 45-FOOT INTERVALS AND AT ALL COLD JOINTS.
2. CONTRACTION JOINTS SHALL BE SPACED THE APPROXIMATE SAME DIMENSION AS THE WIDTH, BUT NOT TO EXCEED SIX FEET. CONTRACTION JOINTS SHALL BE CONSTRUCTED BY SAWING OR SCORING. A TOOL SHALL BE USED WHICH WILL LEAVE THE EDGES ROUNDED AND DESTROY AGGREGATE INTERLOCK FOR THE SPECIFIED MINIMUM DEPTH. CONTRACTION JOINTS SHALL BE A MINIMUM OF 1/4 OF THE TOTAL DEPTH OF THE CONCRETE.
3. ALL VISIBLE EDGES AND JOINTS SHALL BE ROUNDED WITH AN EDGING TOOL WITH A MINIMUM 1/4" RADIUS.
4. CONCRETE SHALL BE M-4000 WITH 3/4" MAXIMUM AGGREGATE, MINIMUM 28-DAY STRENGTH OF 4000 PSI, $6\% \pm 1\frac{1}{2}\%$ AIR ENTRAINMENT, AND MAXIMUM SLUMP OF 4".
5. SIX INCHES OF CRUSHED GRAVEL BASE MATERIAL, -3/4" DIAMETER IS REQUIRED FOR THE SIDEWALK FOUNDATION. THE BASE MATERIAL SHALL BE COMPACTED TO 95% DENSITY ($\pm 3\%$ OPTIMUM MOISTURE) PER AASHTO T-99.
6. SIDEWALK THICKNESSES:
RESIDENTIAL: 4"
COMMERCIAL: 6"

All sidewalks shall #4 bars 12" on center each direction.

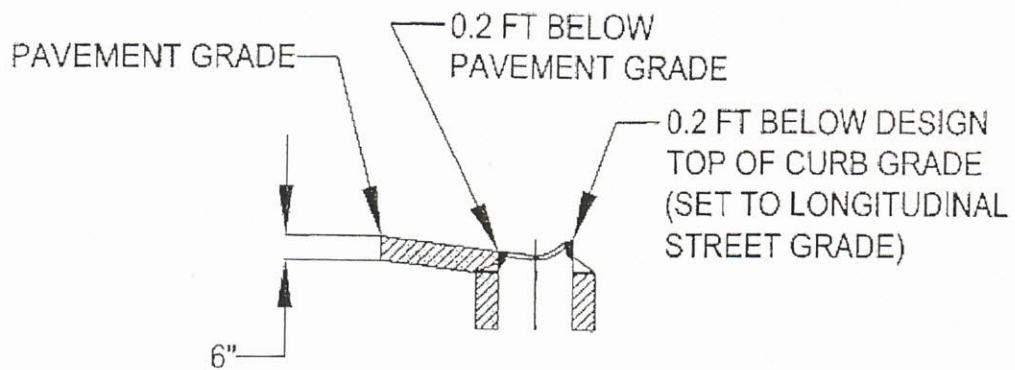
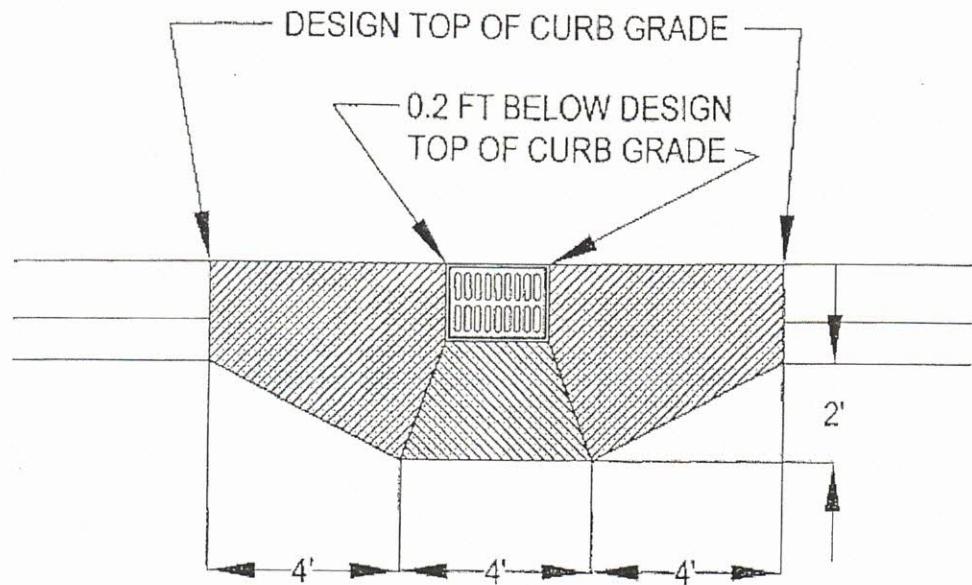
CONSTRUCTION STANDARDS CITY OF LIBBY	SIDEWALK	
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1. PEDESTRIAN RAMPS SHALL COMPLY WITH THE AMERICANS WITH DISABILITIES ACT/ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES.
2. IN INSTANCES WHEN IT WILL BE TECHNICALLY INFEASIBLE FOR A PEDESTRIAN RAMP TO BE CONSTRUCTED TO FULL AND STRICT COMPLIANCE WITH ADA STANDARDS, THE PEDESTRIAN RAMP MUST BE INSTALLED TO PROVIDE ACCESSIBILITY TO THE MAXIMUM EXTENT FEASIBLE. ALTERNATIVE DESIGNS SHALL BE APPROVED BY THE PUBLIC WORKS DEPARTMENT PRIOR TO CONSTRUCTION.

CONSTRUCTION STANDARDS CITY OF LIBBY	RETROFIT PEDESTRIAN RAMP	5
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1. REINFORCE CONCRETE WITH 10/10 6 X 6
WWF SUPPORTED WITH #3 REINFORCING
BARS AT 48" ON CENTER EACH WAY ON 3"
HIGH CHAIRS

2. CONSTRUCT PRIOR TO PAVING.

CONSTRUCTION STANDARDS
CITY OF LIBBY

CONCRETE CURB INLET APRON